

STON WOODS



**TIMMONS GROUP**

YOUR VISION ACHIEVED THROUGH OURS.

ENGINEERING FIRM:

**TIMMONS GROUP**

5401 TRINITY RD, SUITE 102

RALEIGH | NC 27607

WIL ROS MEADOWS



**PUD SUBMITTAL**  
PREPARED FOR:  
TOWN OF KNIGHTDALE  
JULY, 2022

ROBERTSON CROSSING

## Our Mission

### AN INSIGHT FROM TOM NATELLI (PRESIDENT AND CEO)

*Thoughtful Planning, Timeless Design, and Attention To Detail.* These concepts in large part capture the essence of our company's operating philosophy. After having developed properties for over 40 years now, we've come to believe there are fundamental principles that should drive our development efforts, if we are to achieve our goal of creating great places that home buyers and business owners will seek out over all others.

**| AT THE OUTSET** - Because we develop large-scale communities, we realize that we bring tremendous change to the local communities that were already there before we arrived. We realize that it's important to hear from those living and working in these communities at the outset, so we can better understand their concerns and observations and so we can incorporate their suggestions into our development plans, to the extent reasonably possible. If we can do it better than we thought, we ought to consider it. If we can reasonably mitigate the impacts, we ought to accommodate it.

**| IT WILL BE THERE FOR A LONG, LONG, TIME** - We also realize that what we design and implement will ultimately become part of the built environment that others will experience (for better or for worse), for perhaps the next 75 or 100 years, until redevelopment occurs sometime in the distant future. We have the opportunity to design our communities thoughtfully and to execute them well. Frankly, given the significant role we play in helping to create the fabric of the local communities in which we operate, we have an obligation to those that are there now, and to those that come after us, to do it well.

**| THE MOST IMPORTANT INVESTMENT** - To move into one of our communities means that an individual or family has made a life decision to choose an environment created by us in which to raise a family, share a home, shop, work, relax, enjoy, simply live. This is a fundamental decision that bears on almost all aspects of a person's life. It's vital to them, it's vital to us.

**| FIND THE RIGHT BALANCE** - Trends come and go, but true quality is lasting. Some innovations in design and materials make sense, and some come at too great a price, too great a compromise to the quality of the built environment. Take a thoughtful approach to preserving the environment and to finding balance between the needs of our residents and appropriate preservation techniques and systems.



Together with his late father, Anthony M Natelli, Tom Natelli has built Natelli Communities into one of the most respected and accomplished privately held real estate development concerns in the nation. By focusing on excellence in all aspects of design, development, construction and managerial processes, Natelli Communities has consistently developed the best-selling master planned communities in the markets where we operate.

*The company is well versed in local and regional planning, environmental and development matters, and principally acquires unimproved, un-entitled property in the Mid-Atlantic and Southeast region for long-term development.*

Natelli Communities develops a vision for each property, seeks the input of local communities and other interested parties, undertakes the rezoning and entitlement processes and physically develops the properties into finished communities. Residential, retail and office components are sold to national and regional builders under coordinated programs that govern the type, style and level of finish of each of the products constructed by third parties. Natelli builds some of the products through its affiliates, Main Street Homes, Main Street Commercial and Natelli Homes.

Natelli Communities has developed over 15,000 homesites and several million square feet of office and retail space. Communities range in size from several hundred to several thousand units. The company has received dozens of local and national awards including the "Award for Excellence" for Avenel in 1996 for best planned community in the nation by the Urban Land Institute, and "Best Community in the Nation" by the National Association of Homebuilders for Bay Forest in 2018 as well as Washington Metro Environmental Developer of the Year and Suburban Maryland Builder of the Year.

## Vision & Intent

Section 12.2.F.3.g of the Knightdale Unified Development Ordinance states that a Planned Unit Development Overlay district rezoning is a privilege and shall be considered by the Town based on the aggregated incremental benefits that the specific subdivision plans provide that over and above the standard zoning requirements. In return for the flexibility that the Planned Unit Development designation, Robertson Crossing design has been based on the following:

### KnightdaleNext 2035 Comprehensive Plan

- Robertson Crossing is located within the Targeted Investment Area of the Growth Framework Map in the 2035 Comprehensive Plan, and as such, the design encourages active living with a comprehensive network of walkable streets. Compact design is a feature of the community, though, not at the expense of natural and large outdoor gathering areas. Incorporating a Commercial Mixed-Use area further contributes to the walkable lifestyle within one community.
- Consistent with the Growth and Conservation Map, Robertson is predominately located within the Mixed-Density Neighborhood Placetype with a small portion of Rural Living Placetype. Robertson embraces the Mixed-Density Placetype with a significant mixture housing types and densities. Product types differ throughout the development with larger homes adjacent to the many natural preservation areas and smaller lots and townhomes more centrally located adjacent to collector streets. Multiple dwelling types, lot sizes and price points ensure diversity and appeal to a broad array of home buyer. The neighborhood contains a network of open spaces, trails, and natural preservation areas, a primary feature of Mixed-Density Neighborhoods.

### Public Welfare

- Maintaining generous natural buffers and untouched green spaces, Robertson Crossing is designed to not impair the supply of light and air to adjacent properties and will not substantially increase the danger of fire or otherwise endanger the public health, safety and welfare.

### Impact on Other Property

- Robertson Crossing will ensure compatibility with surrounding land uses as a residential and commercial mixed-use development located in a growing area of the Town of Knightdale. Robertson Crossing is a natural extension of Knightdale Station, The Collection and Harper Preserve. The development has coordinated with other future developments adjacent to the property and will not impede the use and future development of surrounding properties nor will it diminish property values of the area.

### Impact on Public Facilities and Resources

- Robertson Crossing is designed to ensure adequate utilities, road connections, drainage and other facilities are existing or will be constructed with the development.

### Archaeological, Historical or Cultural Impact

- Numerous natural features found on the property will be conserved. Several existing ponds will remain so wildlife will continue to flourish. Stream buffers and connected wetland areas provide a conduit for continued wildlife movement. Not only are these elements retained for the enjoyment of the residents, but they are also fully integrated into the design of the development, creating a sense of space unique to Knightdale.
- Robertson Crossing includes traditional modern high-end amenities such as pool, clubhouse, tot lot, parks with seating, dog park, community gardens, pickleball and bocce ball courts, but also preserves and incorporates natural features found on the property like wetlands, streams, and existing ponds. Trails not only enable pedestrian connectivity throughout various sections of the community but serve as conduits for residents to enjoy the natural elements distributed throughout. Active open spaces are flexible community gathering spaces accommodating a wide array of uses.

### Parking and Traffic

- Necessary parking and road connections are proposed with Robertson Crossing to minimize traffic congestion on existing public streets and provide adequate access for emergency vehicles. Robertson Crossing includes a public greenway along Poplar Creek in accordance with Knightdale's Trails and Greenway plan. The greenway will help Knightdale move closer towards making their vision for a pedestrian-friendly community a reality and will create a healthier and more dynamic community by improving pedestrian safety and connectivity.
- The layout of Robertson Crossing seamlessly integrates various dwelling types into the beautiful landscape and a series of collector roads that fulfill the Transportation Plan's goal of connecting parts of southeastern Knightdale. Coordinating with proposed development on adjacent parcels ensures that Robertson Crossing fits into a larger design language for the area. With the completion of these roads, Knightdale is one step closer to the intended transportation system of integrated grid street network for major roads and local streets.

### Adequate Buffering

- Landscaping and open space areas are proposed throughout Robertson Crossing for the enjoyment of residents and passerby. Adjacent property is a mixture of existing and proposed residential development, Knightdale Station Park and agricultural uses. The site is divided by streams and wetlands creating natural buffering.

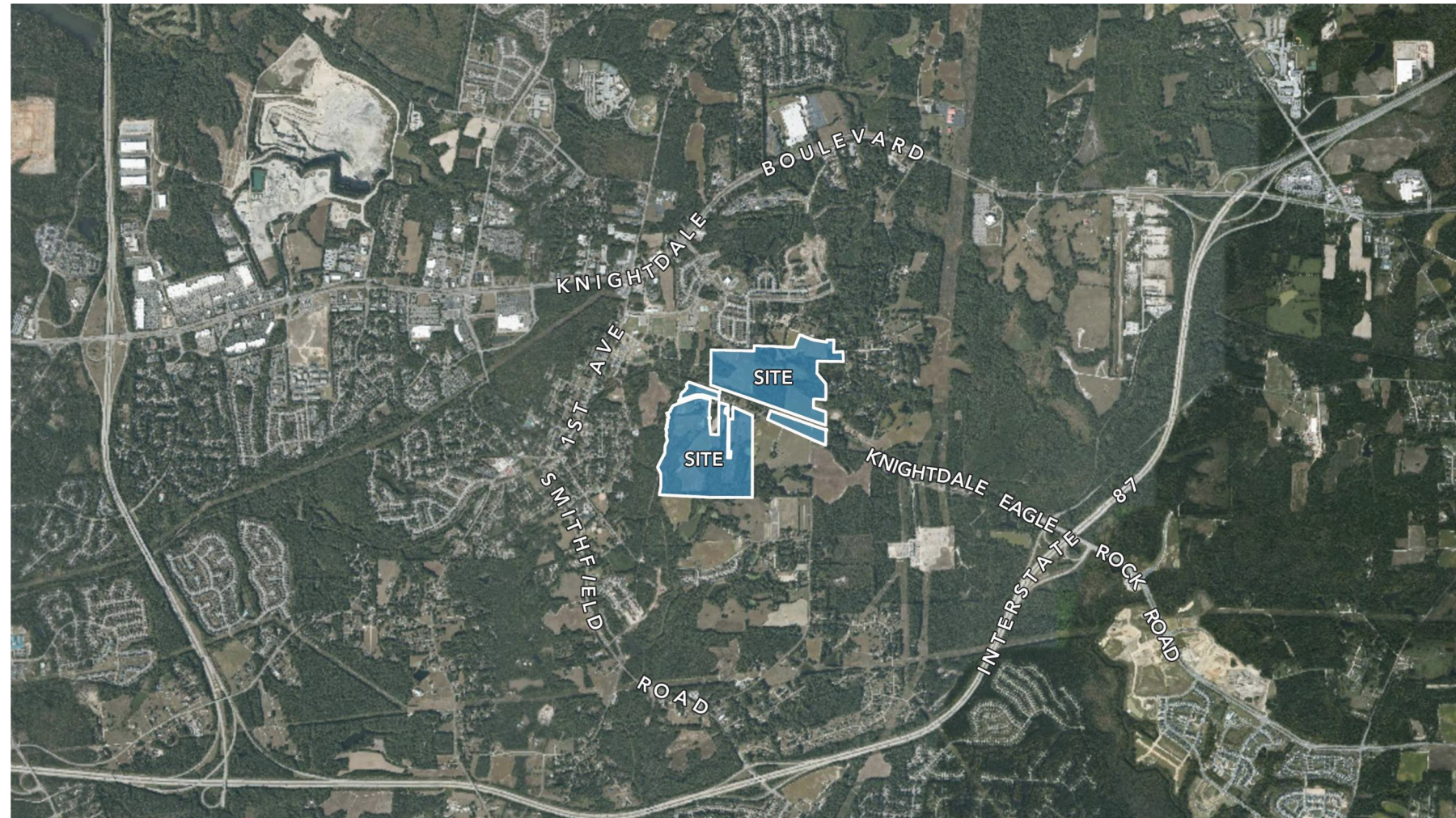
### Performance

- The applicant has a successful history with the Town of Knightdale in developing Allen Park as well as developing projects throughout the Mid-Atlantic and Southeast regions. The applicant is working closely with potential builders to ensure that Robertson Crossing is the highest quality development.

**SITE DATA TABLE**

PROJECT:	ROBERTSON
ENGINEER:	TIMMONS GROUP BETH BLACKMON, PE 5410 TRINITY ROAD, STE. 102 RALEIGH, NC 27607 PHONE: (919) 866-4509 FAX: (919) 859-5663 EMAIL: BETH.BLACKMON@TIMMONS.COM
DEVELOPER:	NATELLI COMMUNITITES BRIAN MASSENGILL 1903 N. HARRISON AVE. CARY, NC 27513 PHONE: (919) 888-3102 EMAIL: BRIAN@NATELLI.COM
LAND OWNER:	POOLE, JAMES HOWARD JR, JAMES HOWARD III 52 BLEEKER DR CLAYTON, NC 27527  HILTON WEATHERS 170 ALMONT ST MATTAPAN MA 02126  ANGEL R & BRIAN K DUNN 3537 MEADOW RIDGE DR RALEIGH NC 27604  HILTON PERRY & ELOISE WEATHERS 170 ALMONT ST MATTAPAN MA 02126  DENNIS PERRY 42 COLSON DR GARNER NC 27529  DON V & DEBORAH J DEBNAM 7905 KNIGHTDALE BLVD KNIGHTDALE NC 27545  TMW FARMS LLC 817 RUNNYMEDE RD RALEIGH NC 27607  DON DEBNAM 152 MAILMAN ROAD KNIGHTDALE NC 27545
TAX ID:	1754-83-4965, 1754-72-7141, 1754-81-0838, 1754-81-2900, 1754-81-4716, PORTION OF 1754-92-3372, 1754-93-6968, PORTION OF 1754-94-5547, 1764-04-3505
DEED REFERENCE:	DB15997 PG77; DB6710 PG212; DB16725 PG1685; DB7793 PG 111; DB12952 PG1020; DB11753 PG2219; DB18454 PG1661; DB12-E PG200
TOWNSHIP:	ST. MATTHEW'S
EXISTING ZONING:	RT & GR3
CURRENT USE:	VACANT & AGRICULTURE
PROPOSED USE:	SINGLE-FAMILY RESIDENTIAL & COMMERCIAL MIXED USE
PROPOSED ZONING:	GR-8 PUD GENERAL RESIDENTIAL-8 (GR-8), PUD (PLANNED UNIT DEVELOPMENT)
TOTAL PROJECT AREA:	188.0 ACRES
PROPOSED DENSITY:	3.55 DU/A
RIVER BASIN:	NEUSE
SURFACE WATER CLASSIFICATION:	C; NSW
WATERSHED:	NONE
FIRM PANEL #:	3720175400K EFF. 06/19/2020

FRONT LOAD SINGLE FAMILY LOT STANDARDS:	MINIMUM LOT SIZE: 6,900 SF MINIMUM LOT WIDTH: 60' FRONT SETBACK: 10' DRIVEWAY LENGTH: 20' SIDE SETBACK: 5' CORNER SIDE SETBACK: 10' REAR SETBACK: 20'
REAR LOAD SINGLE FAMILY LOT STANDARDS:	MINIMUM LOT SIZE: 3,400 SF MINIMUM LOT WIDTH: 34' FRONT SETBACK: 10' SIDE SETBACK: 3' CORNER SIDE SETBACK: 10' REAR SETBACK: 20' DRIVEWAY LENGTH: 20'
REAR LOAD TOWNHOME LOT STANDARDS:	MINIMUM LOT SIZE: 1,980 SF LOT WIDTH: 22' FRONT SETBACK: 10' BUILDING SEPARATION: 10' REAR SETBACK: 20' DRIVEWAY LENGTH: 20'



**SETBACKS AND DRIVEWAYS**

Section 3.4 of the Knightdale UDO requires that any lot less than 80' in width be accessed via an alley or rear lane access. Additionally, the Section 6.5 of the UDO requires that side setbacks be a minimum of 20% of the lot width (to a minimum of 6'), and driveway length be a minimum of 35'. An exception to these requirements is requested to allow the minimum lot width standard to be reduced to 60', side setbacks be reduced to 5', rear setback be reduced to 20', and driveway lengths be reduced to a minimum of 20' to be adequately developed within the site constraints. Per Section 3.4, Knightdale requires townhomes to have a minimum driveway length of 35'. An exception to this requirement is requested to allow townhome driveways to be reduced to 20'. These exceptions are requested based on the Beneficial Common Open Space, Landscape Conservation, Visual Enhancement, and Integrated Design Modification Standards. Compact development better protects the natural environment, increasing the amount of open space and creating a more integrated design for the community.

**GENERAL SITE MODIFICATIONS**

9.3.B of the knightdale unified development ordinance prohibits mass grading on all front-loaded lots 60' or greater in width. All rear loaded units included in this PUD are exempt from this requirement by right, due to their narrow lot width. The single family lots not exempt, include 30% of the total number of lots. An exception to these requirements is requested to allow for the entire development to be eligible for mass grading, rather than only portions of the development. This exemption would allow for a more efficient construction process. This modification is requested based on the Surface Water Drainage Modification Standard to ensure proper site surface drainage is maintained. Collecting runoff does not adversely impact neighboring properties is the basis for this request.

**DISTRIBUTION OF USES**

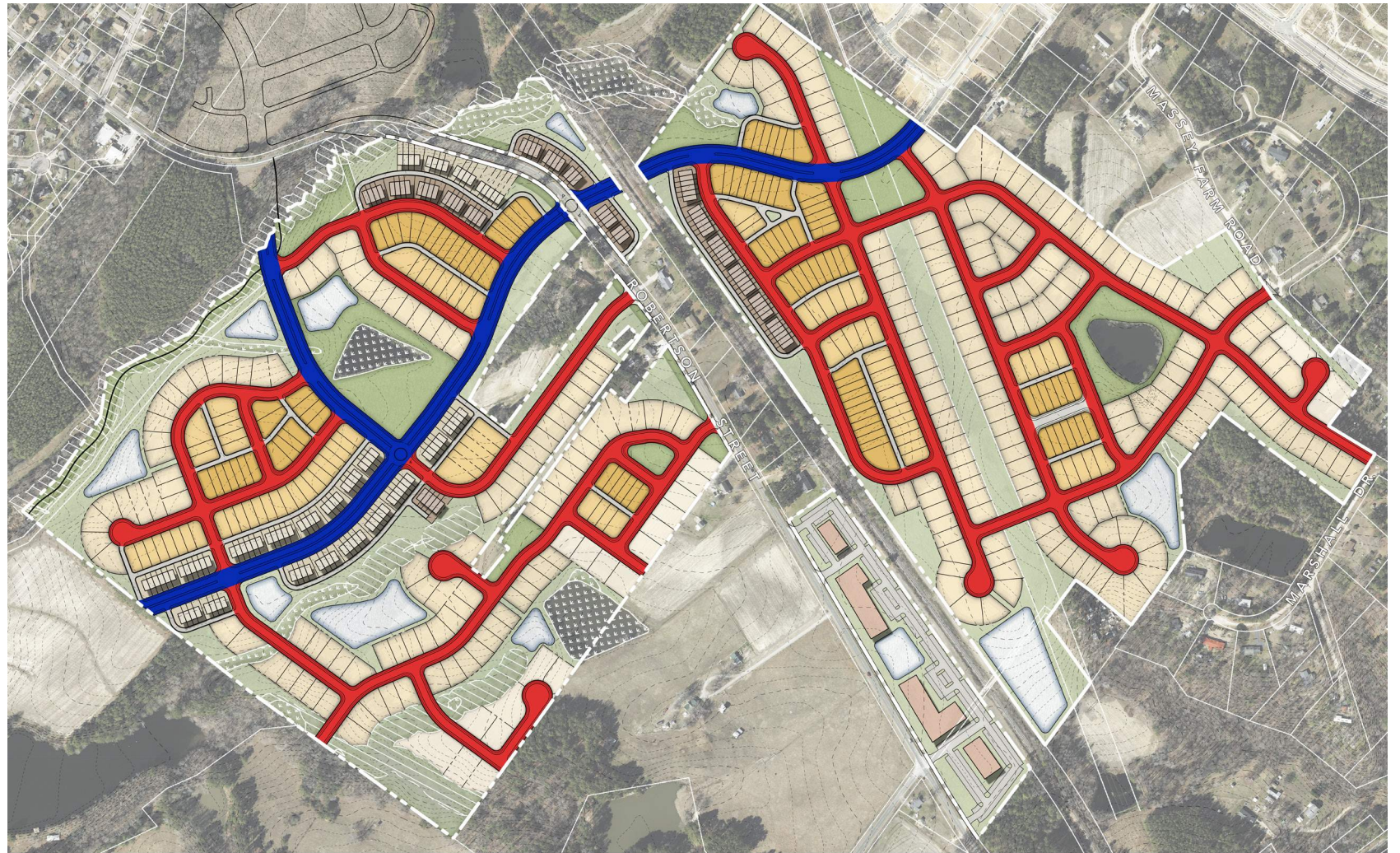
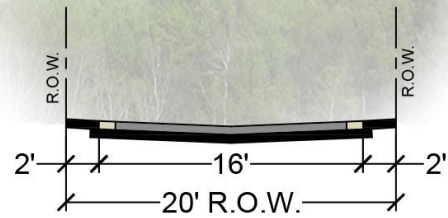
Table 11.1.B in the Knightdale UDO requires a Min. 10% / Max. 40% Duplex / Townhouse, Min. 5% / Max. 40% Multifamily, Min. 15% / Max. 60% Single Family, and Min. 10% / Max. 100% Mixed Use. An exception to these requirements is requested based on the Integrated Design Modification Standard to exclude the requirement for Multifamily use and an increased percentage of single family to provide a safe, efficient and harmonious grouping of uses within this area of Knightdale. A Mixed-Use area has been incorporated however no multifamily is proposed. Proposed distribution of uses is: 26% Townhouse, 64% Single Family, 10% Mixed-Use.

SITE DATA:	
TOTAL AREA	188.0 +/- AC
R.O.W. RESERVATION	0.9 +/- AC
NET AREA	187.1 +/- AC
COMMERCIAL MIXED-USE	8.5 +/- AC
20' X 90' TOWNHOMES	78
22' X 90' TOWNHOMES	98
30' X 120' REAR-LOAD SINGLE FAMILY	110
38' X 110' REAR-LOAD SINGLE FAMILY	85
55' X 110' FRONT-LOAD SINGLE FAMILY	275
80' X 110' FRONT-LOAD SINGLE FAMILY	21
TOTAL UNITS	667
PASSIVE OPEN SPACE REQUIRED	6.5 +/- AC
PASSIVE OPEN SPACE PROVIDED	6.7 +/- AC
ACTIVE OPEN SPACE REQUIRED	6.5 +/- AC
ACTIVE OPEN SPACE PROVIDED	7.7 +/- AC



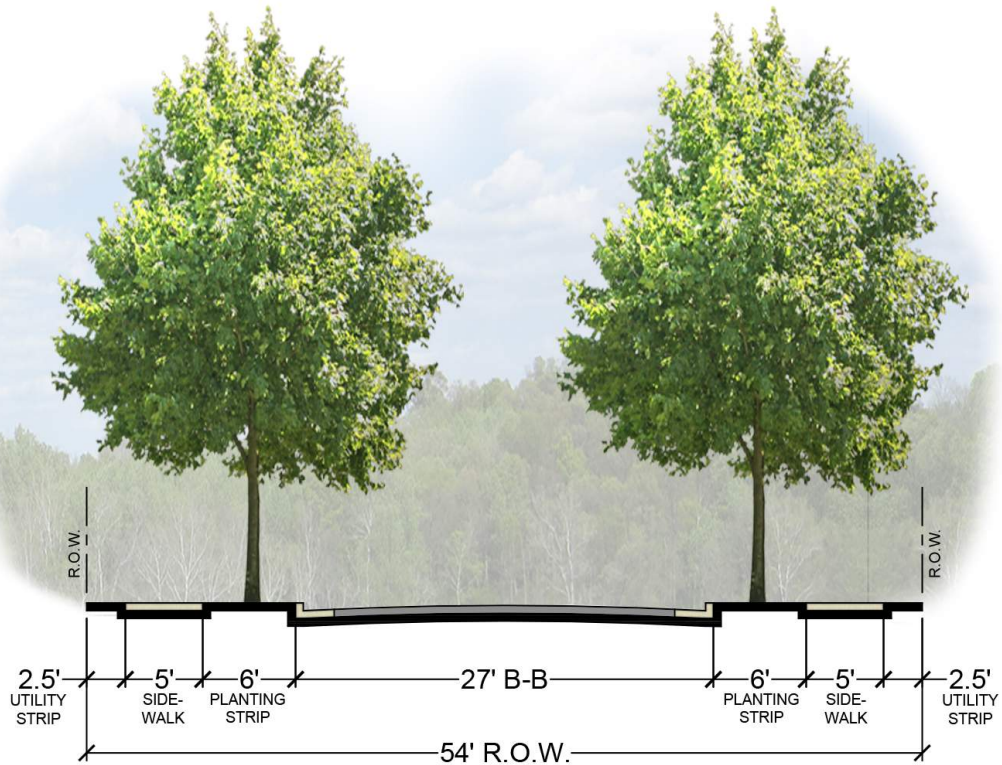
# COMMUNITY MASTER PLAN

TYP. RESIDENTIAL ALLEY

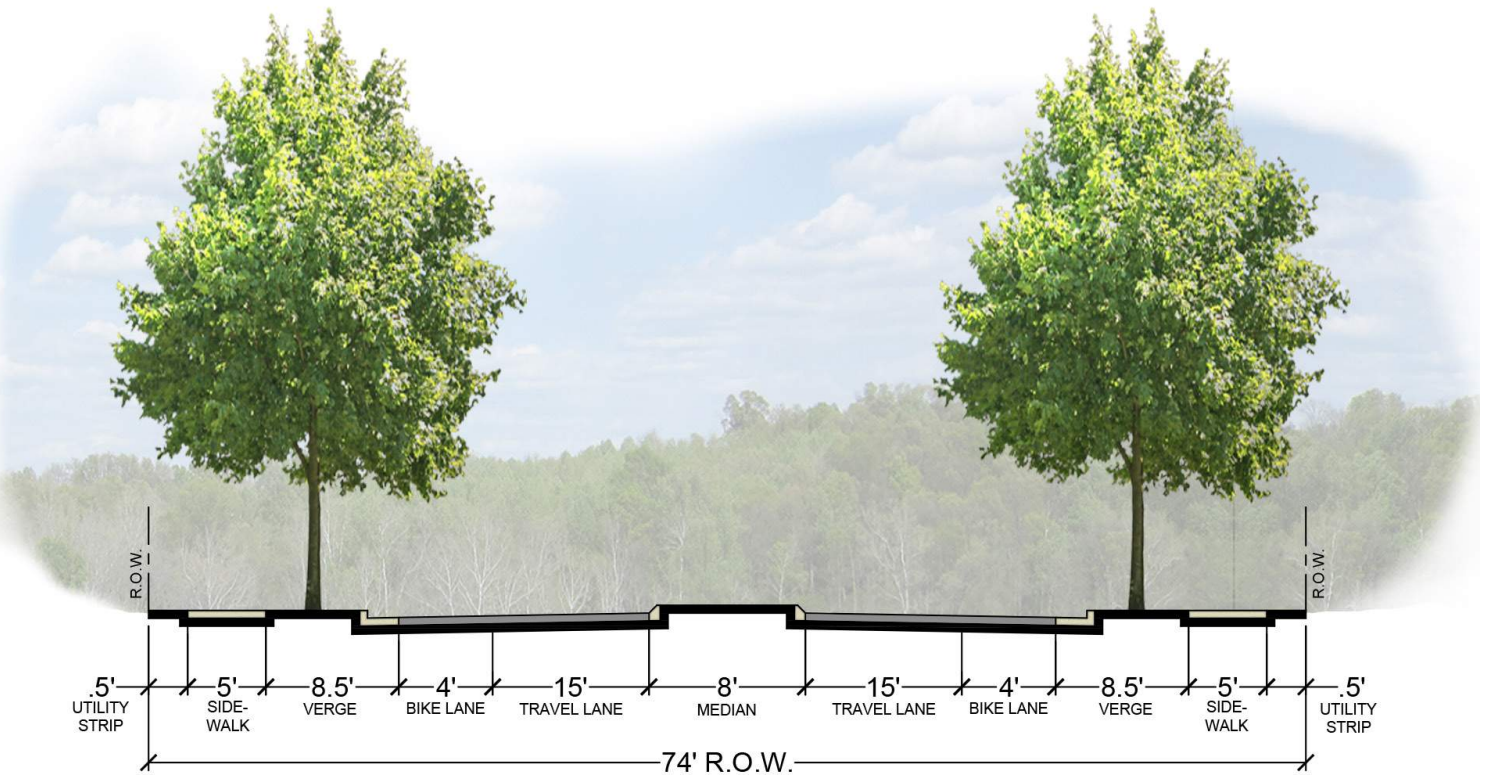


STREET SECTIONS

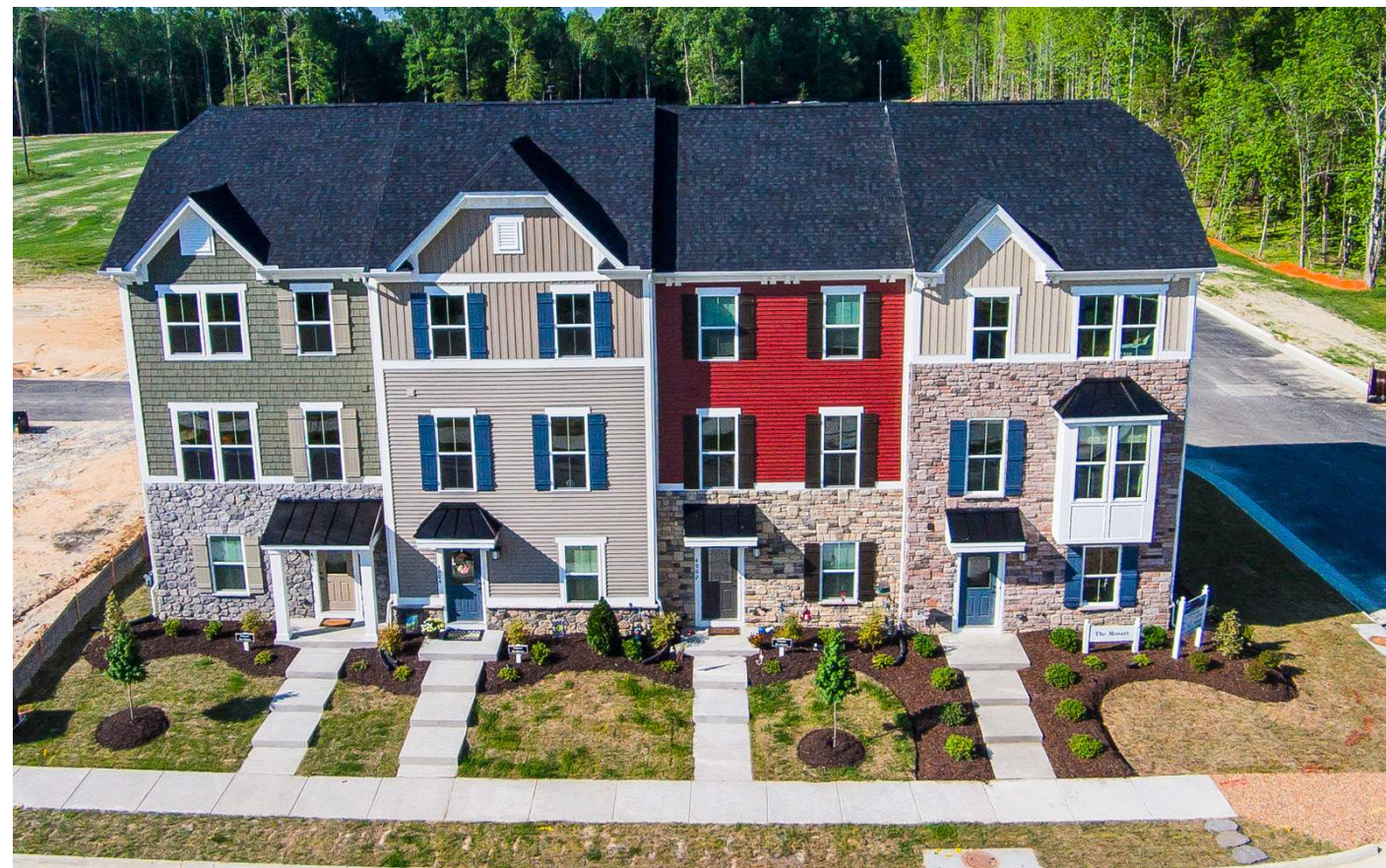
SECTION 1



SECTION 2



# TOWNHOMES - 20' X 90' LOTS



RESIDENTIAL PRODUCT



TOWNHOMES - 22' X 90' LOTS



RESIDENTIAL PRODUCT

**REAR-LOAD SINGLE FAMILY HOMES - 30' X 120'**



RESIDENTIAL PRODUCT

# REAR-LOAD SINGLE FAMILY HOMES - 38' X 110'



RESIDENTIAL PRODUCT

## FRONT-LOAD SINGLE FAMILY HOMES - 55' X 110'



RESIDENTIAL PRODUCT

# FRONT-LOAD SINGLE FAMILY HOMES - 80' X 115'



RESIDENTIAL PRODUCT

- Townhomes shall be two and three-story homes with a minimum width of 20' wide and have a minimum heated area of 1,500 square feet. A minimum of fifty (50) townhomes shall be three-story
- Neo Traditional Single family detached homes shall be rear loaded two-story homes built on lots less than 55 feet wide with a minimum heated area of 1,500 square feet.
- Single Family Ranch detached homes shall be built on lots that are at least 55 feet wide, with a minimum heated area of 1,480 square feet, and no more than 25% of the ranch homes may be less than 1,500 square feet.
- Traditional Single family detached homes shall be two-story homes built on lots at least 55 feet wide with a minimum heated area of 2,100 square feet. No more than 10% of the Traditional Single Family detached homes may be homes less than 35' wide.
- All homes will either consist of a single material of brick or stone or will have a combination of 2 or more of the following materials on the front façade (not including foundations): stone, brick, lap siding, fiber cement siding, shakes, or board and batten siding, with side and rear facades of fiber cement siding. When 2 materials are used, the materials shall be different but complimentary colors. Vinyl may only be used of soffits, fascia, and corner bounds.
- All single family detached homes will have front porches with a minimum depth of 5 feet. Townhomes will have front porches with a minimum depth of 3 feet.
- All traditional single family detached homes will have a rear patio or decks a minimum of 10' x 10'. All single family ranch detached homes will have a minimum 6' x 10' covered patio.
- Main roof pitches (excluding porches) fronting the street will be at least 7:12, with the exception of ranch homes in which 6:12 shall be allowed.
- For every 30 feet (or fraction) of continuous side elevation (calculated on a per floor basis), there shall be one window or door added to the side elevations. Any siding break on the side of the home, such as a fireplace, side porch, or wall offsets may be used as an alternate to windows.
- There shall be a minimum 12 inch overhang on every gable end for every home.
- Garages on all neo traditional single family detached homes and townhomes shall be alley fed.
- All front loaded garage doors shall have glass windows and carriage style hardware.
- All single family detached homes shall be raised from the finished grade a minimum of 18" and shall have stem wall or raised slab foundations that shall be covered on all sides with brick or stone. Areas under porches may be enclosed with lattice.
- All single family attached homes shall be raised from the finished grade a minimum of 18" and shall have stem wall or raised slab foundations that shall be covered on all sides with brick or stone. Areas under porches may be enclosed with lattice.
- Any single family homes with a crawl space shall be wrapped in brick, or stone on all sides.
- All homes shall have front door glass inserts.
- Garages on front load single family detached homes shall not protrude more than six (6) feet from the front porch or stoop and all garage doors shall have window inserts and hardware.
- Garage doors for front load single family detached homes shall be 2-car garage doors. The width of the front facade of the garage shall not exceed 60% of the corresponding front facade's total width. A decorative trellis above the garage door will be included on all Single Family Ranch Homes.
- A minimum of twenty (20) Traditional Single Family detached homes shall be constructed with basements or crawl space construction. Potential lots, as identified by the Developer, for basement or crawl space shall not be included in the mass grading operation. Clearing and grading plans for these groups of lots shall be determined and submitted in conjunction with home construction.
- All townhouse units shall provide detailed design elements using at least one (1) of the features from each of the four categories below:
  1. Entrance:
    - Covered stoop
    - Covered porch with 6" minimum width columns
    - Soldier row of masonry element above door
  2. Building Offset:
    - Facade offset
    - Roofline offset
  3. Roof:
    - Dormer
    - Gable
    - Cupola/Tower/Chimney
    - Decorative cornice of roof line (flat roof only)
  4. Facade - at least two (2) of the following elements:
    - Bay window
    - Balcony
    - Porch
    - Shutters
    - Window trim with 4" minimum width
    - Patterned finish (scales, shakes, wainscoting, brick, or stone)

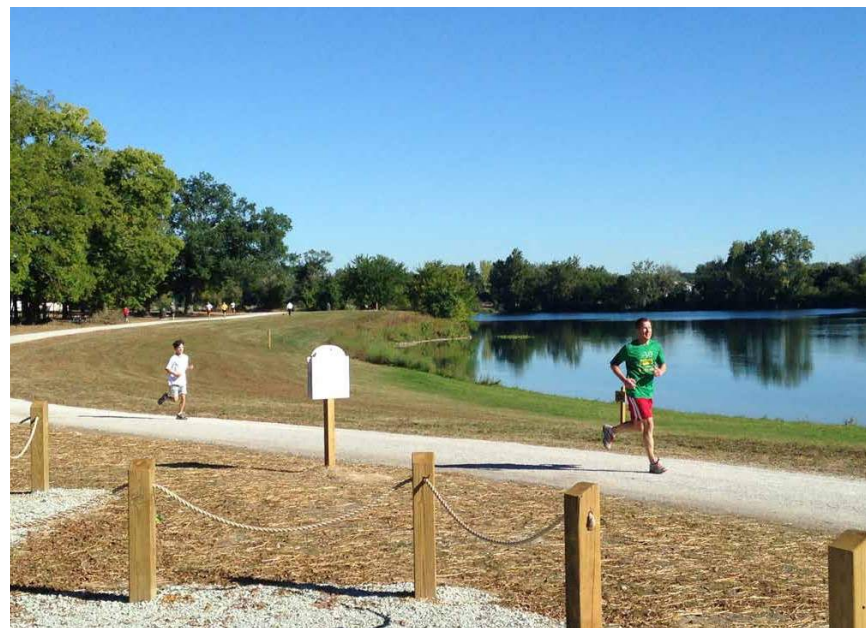


<b>TOTAL BEDROOMS:</b>	
176 TOWNHOME UNITS * 2.5 =	440 BEDROOMS
491 SINGLE FAMILY UNITS * 3.5 =	1,719 BEDROOMS
TOTAL BEDROOMS = 2,159	
<b>OPEN SPACE CALCULATIONS:</b>	
2,159 BEDROOMS * 260 =	561,340 SF REQUIRED
PASSIVE OPEN SPACE REQUIRED =	280,670 SF
PASSIVE OPEN SPACE PROVIDED =	292,626 SF
ACTIVE OPEN SPACE REQUIRED =	280,670 SF
ACTIVE OPEN SPACE PROVIDED =	335,368 SF
<b>CANOPY SAVE:</b>	
CANOPY SAVE REQUIRED:	605,740 SF
CANOPY SAVE PROVIDED:	618,564 SF

# COMMUNITY AMENITIES

**Robertson Allocation Points:**

<b>ITEM</b>	<b>POINTS</b>
Major Subdivision	15
Residential Architectural Standards	15
Pickleball Court	5
Clubhouse without kitchen 1500-2499 sf	5
Resort Style Pool	2
IPEMA Certified Playground Equipment	4
Construct more than 1000 linear feet of 10' wide path	4
Multi-Use Hard Bocce court	5
<b>Total</b>	<b>55</b>



COMMUNITY AMENITIES



